



GALWAY CITY COUNCIL

OLD DUBLIN ROAD - NEW LAWN CEMETERY

STAGE F PART 1 ROAD SAFETY AUDIT

AUGUST 2023



OLD DUBLIN ROAD-NEW LAWN CEMETERY

STAGE F PART 1 ROAD SAFETY AUDIT

Document Control Sheet	
Document Reference	11614 RSA Stage F Part 1
Report Status	Issue
Report Date	August 2023
Current Revision	B
Client:	Galway City Council
Client Address:	City Hall, College Road, Galway
Project Number	11614

Sligo Office Fairgreen House, Fairgreen Road, Sligo, H91 AXK8, Ireland. Tel: +353 (0)91 565 211	Dublin Office Block 10-4, Blanchardstown Corporate Park, Dublin 15, D15 X98N, Ireland. Tel: +353 (0)1 803 0406	Castlebar Office Market Square, Castlebar, Mayo, F23 Y427, Ireland. Tel: +353 (0)94 902 1401
---	--	--

Revision	Description	Author:	Date	Reviewed By:	Date	Authorised by:	Date
A	Issue	LG	25.04.2023	FF	12.05.2023	FF	16.05.2023
B	Issue – 2 no. routes	LG	08.08.2023	FF	09.08.2023	FF	10.08.2023

TOBIN Consulting Engineers

Disclaimer
 This Document is Copyright of TOBIN Consulting Engineers Limited. This document and its contents have been prepared for the sole use of our client. No liability is accepted by TOBIN Consulting Engineers Limited for the use of this report, or its contents for any other use than for which it was prepared.



Table of Contents

1.0	INTRODUCTION	1
1.1	LOCATION.....	1
1.2	AUDIT TEAM.....	1
1.3	AUDIT INFORMATION	2
1.4	DESCRIPTION OF DEVELOPMENT	2
1.5	INFORMATION PROVIDED TO AUDIT TEAM.....	3
2.0	ITEMS ARISING FROM THE AUDIT	4
2.1	OPTION 1: GREEN ENTRANCE (G).....	4
2.1.1	Problem	4
2.1.2	Problem	5
2.1.3	Problem	5
2.1.4	Problem	5
2.1.5	Problem	5
2.2	OPTION 2: RED ENTRANCE (R)	6
2.2.1	Problem	6
2.2.2	Problem	6
2.2.3	Problem	7
2.2.4	Problem	7
2.2.5	Problem	7
3.0	Preference of Design Options.....	8
3.1.1	Green Entrance.....	8
3.1.2	Red Entrance	8
3.2	RANKING OF ROUTE OPTIONS.....	8
4.0	AUDIT TEAM STATEMENT.....	10



Table of Figures & Plates

Figure 1-1 Site Location Map	1
Figure 1-2 Entrance Options.....	3
Figure 2-1 Map Green Entrance.....	4
Figure 2-2 Map Red Entrance	6

Appendices

Appendix A – List of Documents Examined



1.0 INTRODUCTION

1.1 LOCATION

This report describes Part 1 of a Stage F, Feasibility Road Safety Audit, carried out on the proposed entrance to New Lawn Cemetery on the Old Dublin Road (R338) in Galway City, see Figure 1-1.



Figure 1-1 Site Location Map

1.2 AUDIT TEAM

The audit team members were as follows:

- | | | |
|-------------------|----------------------------|---------------------------------|
| • Francis Fidgeon | CST Group | Team Leader TII Ref.: FF74289 |
| • Laura Gaffney | Tobin Consulting Engineers | Team Member TII Ref.: LG3386505 |
| • Donncha Keohane | Tobin Consulting Engineers | Observer |

1.3 AUDIT INFORMATION

The audit comprised an examination of the drawings supplied by Galway City Council (GCC) and one site visit. The audit site visit was carried out during the day on the 17th of April 2023. The weather conditions were generally dry and bright.

This Stage F Part 1 Road Safety Audit has been carried out in accordance with the requirements of TII GE-STY-01024. The proposed entrance options for the scheme have been examined and this report compiled in respect of the consideration of those matters that may have an adverse effect on road safety. It has not examined or verified the compliance with any other standard or criteria. The overall number and severity of identified problems as well as overall safety considerations of each route option has advised the comparative safety ranking of these schemes in this report.

2 No. entrance were reviewed under this audit. Of the 2 options, both are from the Old Dublin Road (R338). The routes are designated by colour.

The routes audited are:

- Green – Preferred Entrance
- Red – Alternative Entrance No. 1

The audit team has provided an overall relative grading of these route options.

1.4 DESCRIPTION OF DEVELOPMENT

The Development is bounded by the R338, Old Dublin Road, on its south, The Connaught Hotel on its east, Wellpark Grove on its north and a greenfield with pedestrian route linking Wellpark Grove to the R338 on its west.

The development proposes a new entrance to the new development, New Lawn Cemetery. The proposed entrance options are outlined below and shown in Figure 1-2:

- **Green** - via the R338 Old Dublin Road existing entrance link road to the Connaught Hotel.
- **Red** - direct entrance from the R338 Old Dublin Road.

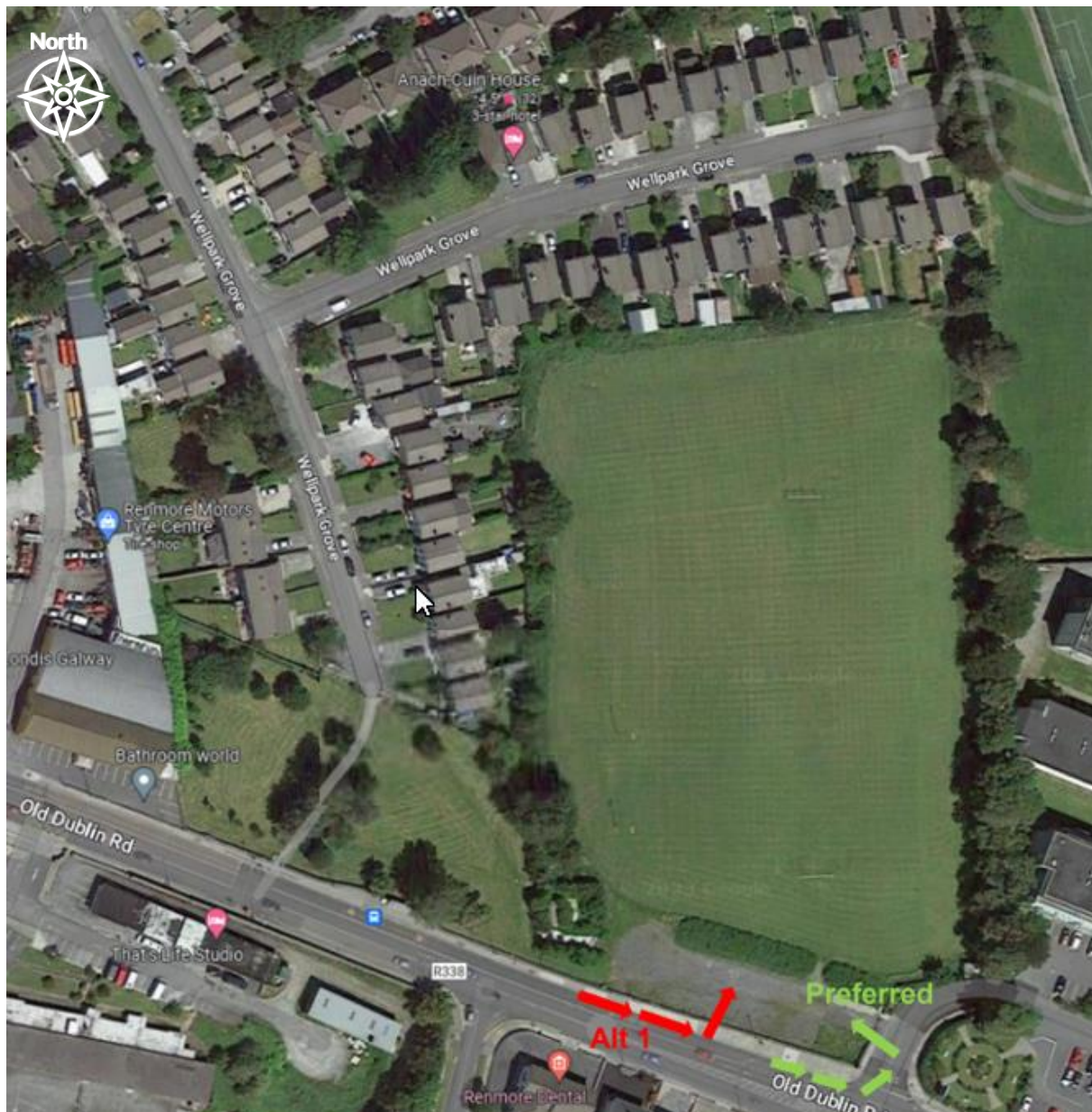


Figure 1-2 Entrance Options

GCC have advised on future road improvements on the R338 (Old Dublin Road) to be considered in the RSA. The Bus Connects project is planned. The proposed Bus Connects will alter the existing road cross section to include for off road segregated footway and cycleway with a bus lane and traffic lane in each direction.

1.5 INFORMATION PROVIDED TO AUDIT TEAM

Drawings detailing the proposed route options were provided. Detail of this information is included in **Appendix A**. Discussions with the design team have taken place in advance and throughout the process of the audit. Two principal options were provided for review.

2.0 ITEMS ARISING FROM THE AUDIT

2.1 OPTION 1: GREEN ENTRANCE (G)

The entrance to the development is located off the link road from the R338 (Dublin Road) to the Connaught Hotel. The proposed entrance is via the existing gate to the development.



Figure 2-1 Map Green Entrance

2.1.1 Problem

The visibility splay from the existing gate to a vehicle travelling eastbound on the Old Dublin Road is obstructed by the existing boundary wall and proposed vegetation.

Hazard

Vehicles turning from the Old Dublin Road into the Connaught Hotel will not have sufficient visibility to react if a vehicle turns out of the new entrance onto the link road between the Connaught Hotel and Old Dublin Road.

2.1.2 Problem

The proximity of the proposed entrance to the mainline traffic on the R338.

Hazard

The distance from a vehicle turning off the mainline (from either direction) to a vehicle departing the entrance / queue of vehicle entering the new entrance, may result in sideswipe / rear-end collision.

2.1.3 Problem

A queue of vehicles on the link road departing the hotel, may present a problem to vehicles departing the development.

Hazard

Queuing vehicles on the link road will obstruct vehicles departing the development. This could result in a vehicle stranded in the inbound lane to the Hotel which may be struck by a vehicle turning into the Connaught hotel from the mainline (R338).

2.1.4 Problem

It was observed a number of pedestrians departing the Connaught Hotel in the direction of town, cross diagonally in front of the new entrance to the development.

Hazard

The lack of pedestrian provisions on northwest side of the proposed entrance will result in a continuation of this existing pedestrian pattern. The introduction of the entrance will present a hazard to these vulnerable road users (VRUs).

2.1.5 Problem

No designated segregated pedestrian access to the development is proposed.

Hazard

In the absence of segregated pedestrian and vehicle access, pedestrians (VRUs) will utilise the proposed vehicular access in conflict with vehicles.

Note, the junction of Old Dublin Road (R338) with the Connaught Hotel Link Road has not been assessed as outside the scope of the works.

2.2 OPTION 2: RED ENTRANCE (R)

The entrance to the development is located off the R338 (Dublin Road) as a new direct access.



Figure 2-2 Map Red Entrance

2.2.1 Problem

The suspension of the bus lane on the eastbound approach to the new entrance and the proximity of the entrance to the Connaught Hotel.

Hazard

The suspension of the bus lane to accommodate merging traffic into the nearside lane to the Cemetery may result in vehicles turning left into the Connaught Hotel entering the nearside lane early. The vehicles departing the Cemetery directly onto the Old Dublin Road may turn out into the oncoming path of these vehicles, resulting in rear-end / sideswipe collisions.

2.2.2 Problem

Proximity of the entrance to the Renmore Park road.

Hazard

The Audit Team observed vehicles queuing on the Old Dublin Road to right turn onto Renmore Park. Vehicles queuing behind were observed undertaking the queued traffic via the bus lane.

This may result in a collision between a vehicle departing the development and a vehicle performing an undertaking manoeuvre which was not anticipated.

2.2.3 Problem

High-sided vehicles queuing to the traffic lights at Renmore Road.

Hazard

Queuing of buses in the bus lane and high-sided vehicles in the offside lane will obstruct the visibility of drivers departing the development to those travelling westbound towards Galway City. This may result in a collision between a vehicle departing and turning right and a vehicle travelling westbound.

A vehicle which accelerates quickly to make a gap acceptance into town from the development, may suddenly brake due to vehicles breaking on the mainline to allow vehicles exit from Renmore Park.

2.2.4 Problem

Boundary wall obstructs the visibility to the new road layout for the Bus Connects.

Hazard

Reduced visibility will increase the risk of the collision and the severity.

2.2.5 Problem

Pedestrians crossing entrance / pedestrian access to the development.

Hazard

No information was provided on a crossing of the entrance on the R338, Old Dublin Road. A bus stop and pedestrian crossing of the R338 is located to the west of the entrance. As a main arterial route in Galway a number of pedestrians use these facilities. The design should accommodate safe movements for these road users.

The placement of this entrance on the R338, will require segregated pedestrian access to the development. Vehicles turning in right with gap acceptance on the mainline, will accelerate into the entrance across the multi-lanes on the R338. High speeds of vehicles in conflict with VRUs may result in serious collisions.

3.0 PREFERENCE OF DESIGN OPTIONS

Following on from the safety concerns outlined in the previous section this is a summary of the main points/issues identified on each option.

3.1.1 Green Entrance

The Green Entrance will have the least impact on the existing operation of the road network. To access the development it will utilise existing road network onto a private link road, requiring no works on the mainline R338 Old Dublin Road. Existing right-turn facilities from the Old Dublin Road are present and suspension of the bus lane for access to the Hotel is present.

A low volume of VRUs will be present in proximity to the junction as the link road provides access to an existing Hotel, Connaught Hotel.

3.1.2 Red Entrance

The Red Entrance will be a new direct entrance from the busy arterial R338 Old Dublin Road. It will require significant changes to the road layout on the R338 to facilitate access /egress from the development.

This option will impact on the existing bus lane on the R338. A higher volume of pedestrians is present on the R338 than in other options. Consideration to safe and segregated access for pedestrians from vehicles will be required.

3.2 RANKING OF ROUTE OPTIONS

The Safety Audit Team carried out a full review of all relevant drawings and documents in relation to the developed route options. In addition, the team visited the site.

The main safety considerations in comparing the routes at this feasibility stage included the location of the entrance, type and arrangement of junctions (i.e. impact on existing road network), vulnerable road users (i.e. pedestrians / cyclists), general impact and interface with the existing network, potential design issues and other potential residual risks.

The summary of some of the comparative items reviewed is given below in Table 3-1.

The comparison of junctions is based on the junction location and sensitivity of the receptors, type and arrangement of the junction(s):

- Junction Location - what is the nature of the receiving environment (i.e. national road, urban / rural / transition, arterial road, local street, residential street, commercial street etc) (Low = not sensitive)
- Types and Arrangement of Junction: Is it a direct access? Or is a link road required? (Low = least impact)
- Where opportunities exist for rationalisation (Low = more opportunity)
- Whether at-grade or grade-separated (Low = more appropriate)
- The VRU impact takes into account pedestrian and cyclist' volumes and impact to their current desire line and increase in interface with vehicles.
 - Low = least impact
 - High = most impact
- The interface with existing network takes into account the impact on the existing road network including:
 - the duration and type of road works (Low = short term / minor works)

- changes to existing road layout (Low = no works)
- The interface with future network takes into account the impact of the Bus Connects Design on the R338.
 - Low = no impact

Table 3-1 Comparative advantages / disadvantages (non-exhaustive / selected)

Option	Ref.	Junction Location	Type and Arrangement of Junction	VRUs Impact	Interface with Existing Road	Interface with Future Road Layout (i.e. Bus Connects)
Green	G	Private Link Road (Low)	Direct access onto existing private access with low traffic volumes to Hotel (Low)	Low VRU volume (Low)	Roadworks on local private road and no changes to existing road layout (Low)	Positive impact with new footway crossing entrance / Right turn pocket removed. (Medium)
Red	R	R338 Arterial Road (Low)	Direct access onto main arterial route into Galway City. (Medium)	Proximity to Bus stop / permeable link to Wellpark (Medium)	Roadworks on a regional road and changes to road layout (R338) (Medium)	Additional distance for vehicles right turning from site (High)

The audit team has concluded that the route option proposals, as provided, rank as shown in the table below in terms of road safety. The ranking shown in Table 3-2 is purely a relative grading of the route options with respect to each other.

Table 3-2 Option Ranking

Option	Ref.	Rank
Green	G	1
Red	R	2

4.0 AUDIT TEAM STATEMENT

We certify that we have examined the design drawings and other information listed in Appendix A and visited the site during the day of the 17th of April 2023. We further certify that we are independent from the design team for the scheme. This examination has been carried out with the sole purpose of identifying any features of the design that could be removed or modified in order to improve the safety of the scheme. The problems that we have identified have been noted in this report, together with suggestions for a preferred option.

AUDIT TEAM LEADER

Name: Francis Fidgeon

Signed:

TII Reference: FF74289

Date: 10th August 2023

Position: Partner

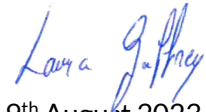
Organisation: CST Group

Address: 1 O'Connell Street,
Sligo

AUDIT TEAM MEMBERS

Name: Laura Gaffney - MSc. BEng (Hons), CEng., MIEI

Signed:



TII Reference: LG3386505

Date: 9th August 2023

Position: Senior Engineer

Organisation: TOBIN Consulting Engineers

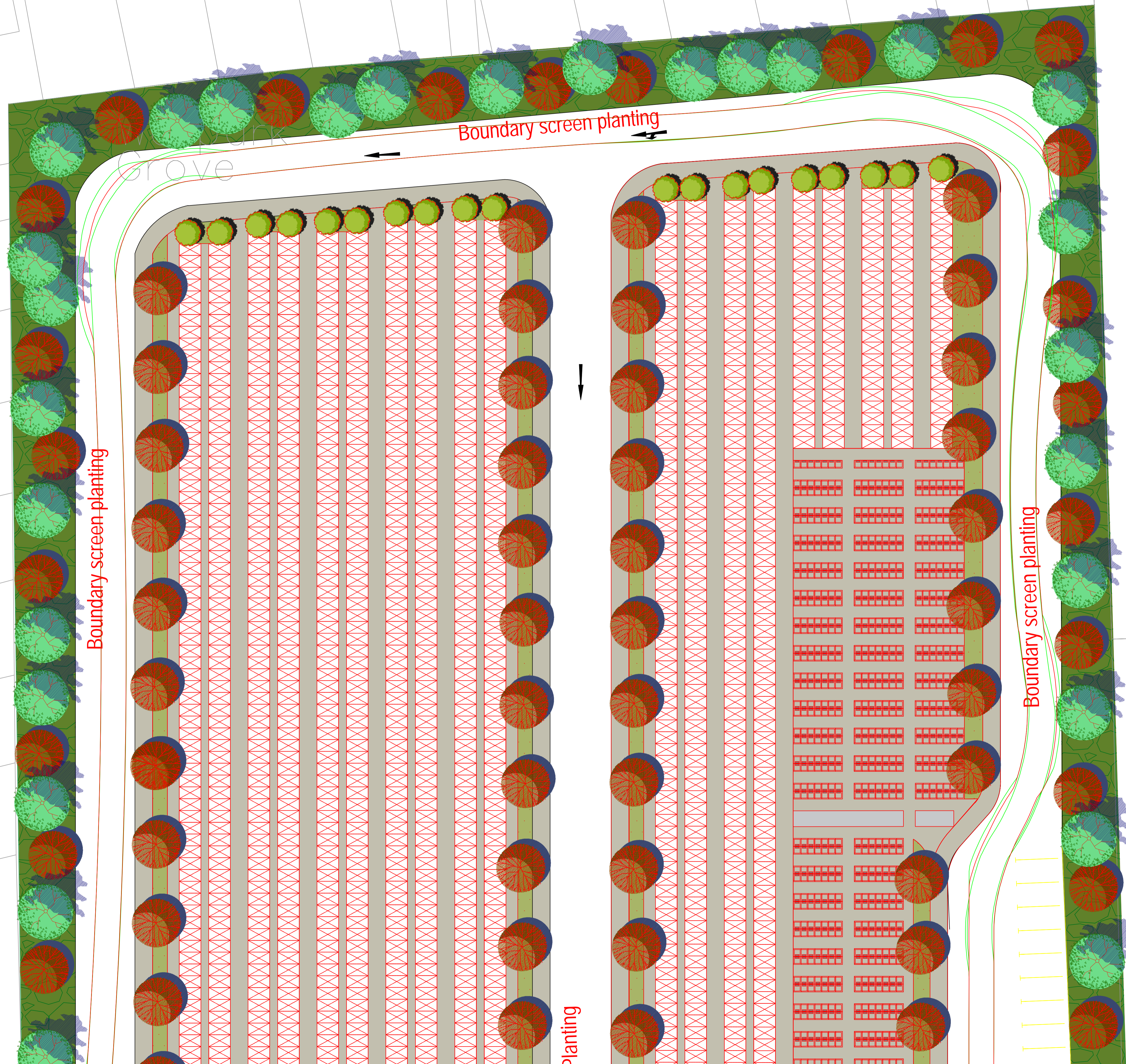
Address: Fairgreen House,
Fairgreen Road,
Galway.






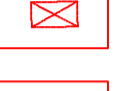


Appendix A – List of Documents Examined

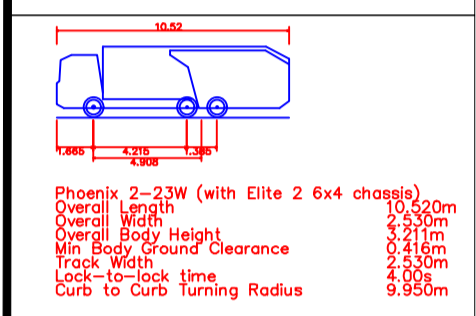
- Feasibility Drawings
 - Masterplan of Options: Preferred Route, Alternative Route 1
 - 11614-1001_D00 Signs, Linemarking and Visibility Splay
 - 11614-1002_D00 Signs, Linemarking and Visibility Splay
- Bus Connects Galway – Dublin Road Drawings
 - 22047-BTL-ZZ-ZZ-DR-CR-06010
 - 22047-BTL-ZZ-ZZ-DR-CR-06009
 - 22047-BTL-ZZ-ZZ-DR-CR-06008
 - 22047-BTL-ZZ-ZZ-DR-CR-06007
 - 22047-BTL-ZZ-ZZ-DR-CR-06006
 - 22047-BTL-ZZ-ZZ-DR-CR-06005
 - 22047-BTL-ZZ-ZZ-DR-CR-06004
 - 22047-BTL-ZZ-ZZ-DR-CR-06003
 - 22047-BTL-ZZ-ZZ-DR-CR-06002
 - 22047-BTL-ZZ-ZZ-DR-CR-06001
 - 22047-BTL-ZZ-ZZ-DR-CR-06000



60



-  Large deciduous tree
-  Large evergreen tree
-  Medium / Small trees
-  Amenity grass
-  Native woodland understorey
-  Grave plots (2.2m x 1.2m)
-  Urn plots
-  Footpaths



- NOTES**
1. FIGURED DIMENSIONS ONLY TO BE TAKEN FROM THIS DRAWING
 2. ALL DRAWINGS TO BE CHECKED BY THE CONTRACTOR ON SITE
 3. ENGINEER TO BE INFORMED BY THE CONTRACTOR OF ANY DISCREPANCIES BEFORE ANY WORK COMMENCES

DOO	APRIL 23	DRAFT	M.C.	B.G.
Rev.	Date	Description	By	Chkd.

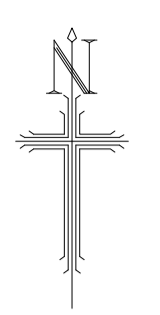
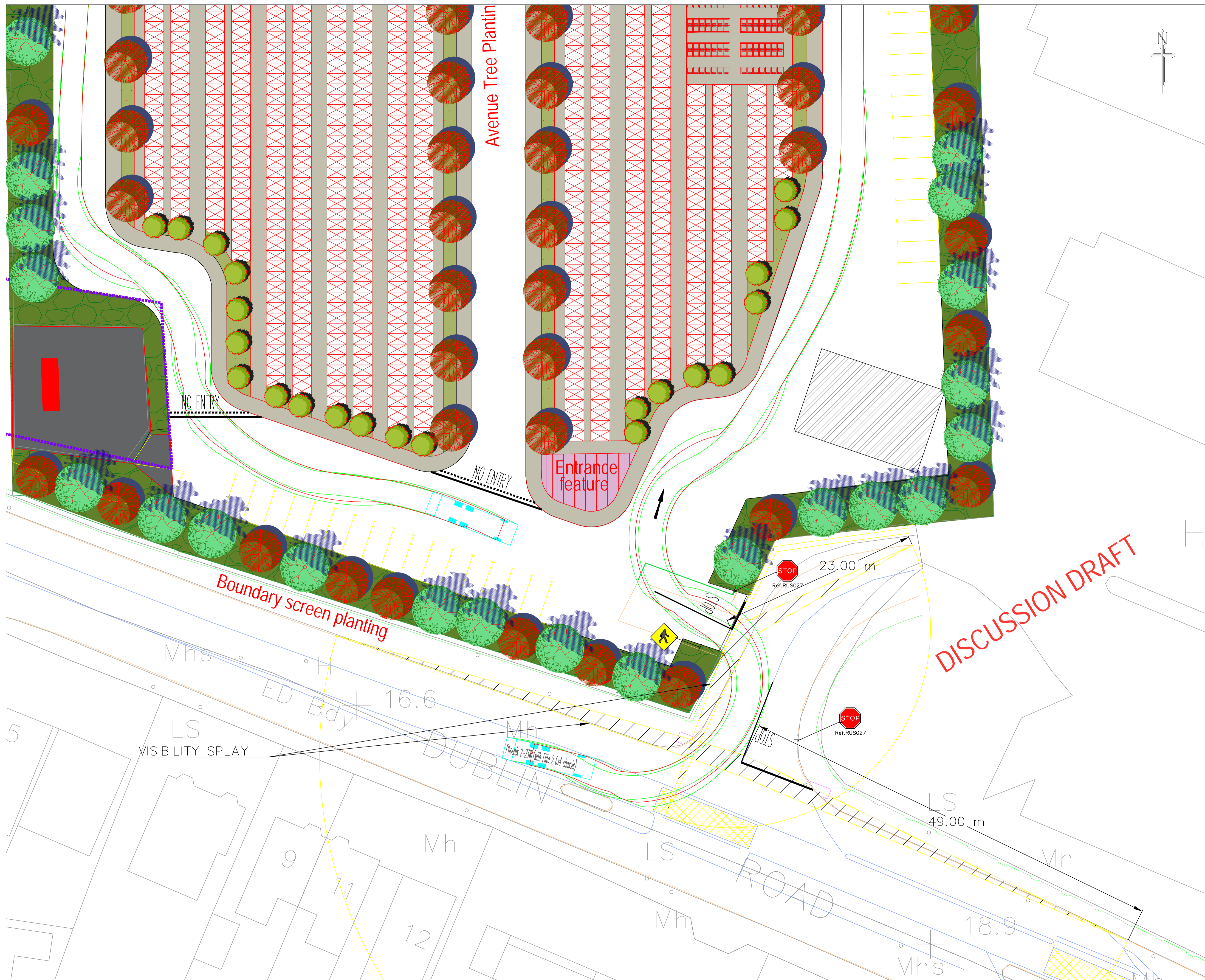
Client
GALWAY CITY COUNCIL

Project
OLD DUBLIN ROAD NEW LAWN CEMETERY

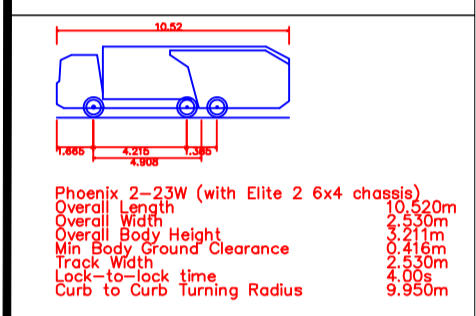
Title
SIGNS, LINEMARKING AND VISIBILITY SPLAYS

Scale	1:250	
Prepared by:	Checked	Date
M.C.	B.G.	APRIL 23
Project Director	B.GALLAGHER	
Drawing Status	DRAFT	

TOBIN
CONSULTING ENGINEERS
Consulting, Civil and Structural Engineers,
Fairgreen House, Fairgreen Road,
Galway, Ireland.
tel: +353-(0)91-565211
fax: +353-(0)91-565398
e-mail: galway@tobin.ie
www.tobin.ie



- Large deciduous tree
- Large evergreen tree
- Medium / Small trees
- Amenity grass
- Native woodland understorey
- Grave plots (2.2m x 1.2m)
- Urn plots
- Footpaths



- NOTES**
1. FIGURED DIMENSIONS ONLY TO BE TAKEN FROM THIS DRAWING
 2. ALL DRAWINGS TO BE CHECKED BY THE CONTRACTOR ON SITE
 3. ENGINEER TO BE INFORMED BY THE CONTRACTOR OF ANY DISCREPANCIES BEFORE ANY WORK COMMENCES

DISCUSSION DRAFT

DOO	APRIL 23	DRAFT					M.C.	B.G.
Rev.	Date	Description	By	Chkd.				

Client
GALWAY CITY COUNCIL

Project
OLD DUBLIN ROAD NEW LAWN CEMETERY

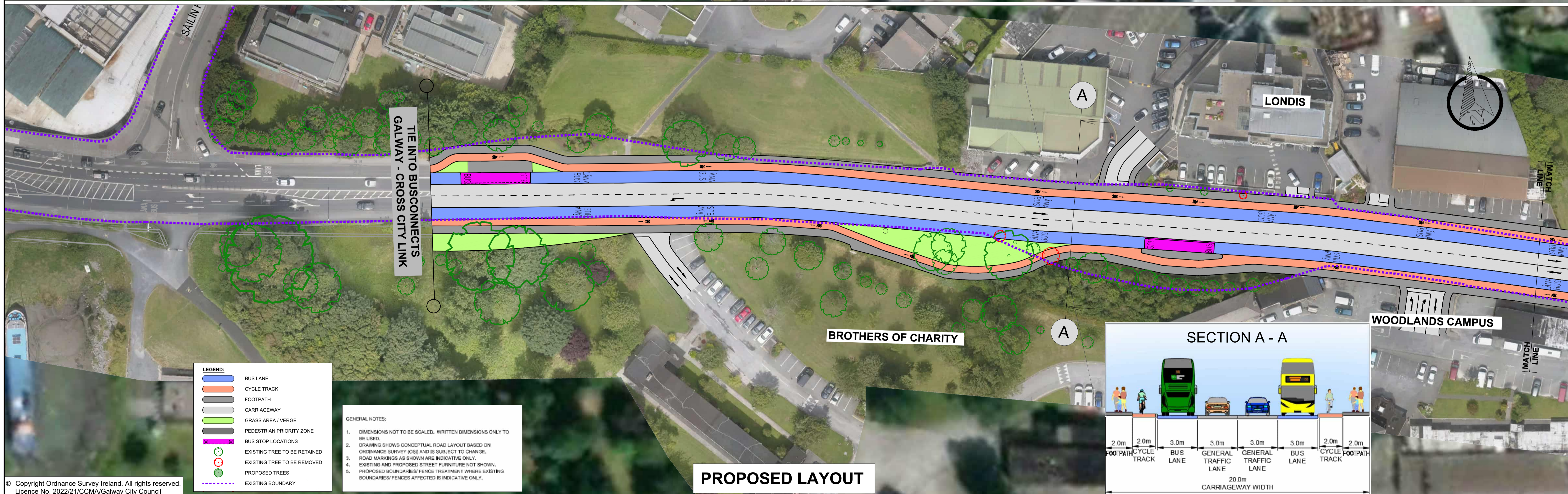
Title
SIGNS, LINEMARKING AND VISIBILITY SPLAYS

Scale	1:250	
Prepared by:	Checked	Date
M.C.	B.G.	APRIL 23
Project Director	B.GALLAGHER	
Drawing Status	DRAFT	

TOBIN
CONSULTING ENGINEERS
Consulting, Civil and Structural Engineers,
Fairgreen House, Fairgreen Road,
Galway, Ireland.
tel: +353-(0)91-565211
fax: +353-(0)91-565398
e-mail: galway@tobin.ie
www.tobin.ie



EXISTING LAYOUT



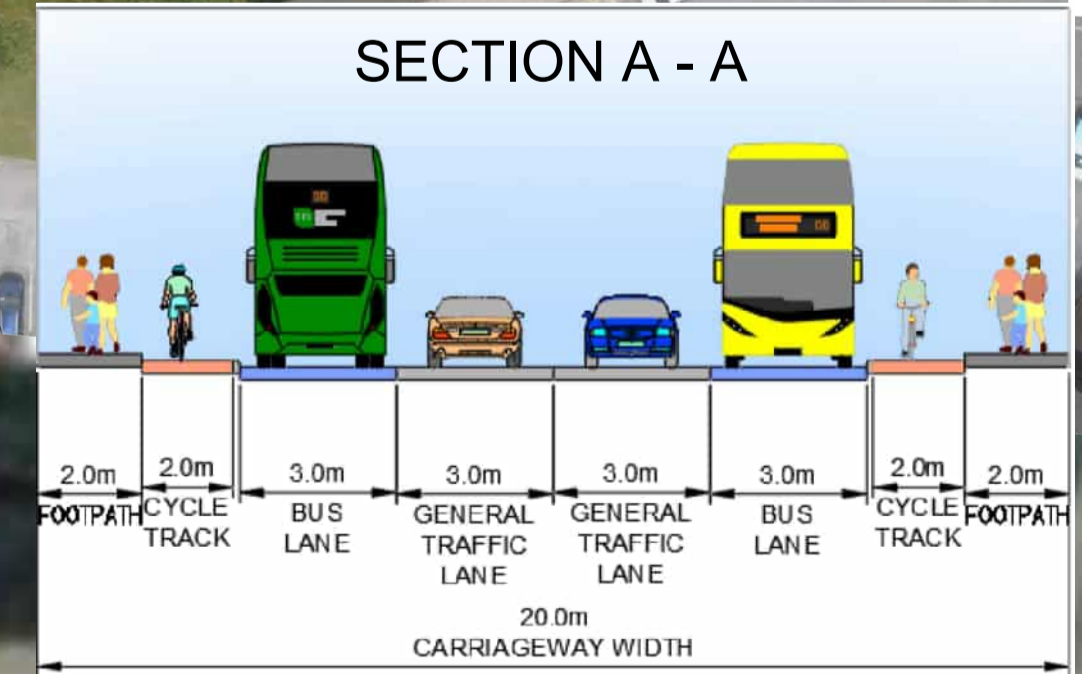
PROPOSED LAYOUT

LEGEND:

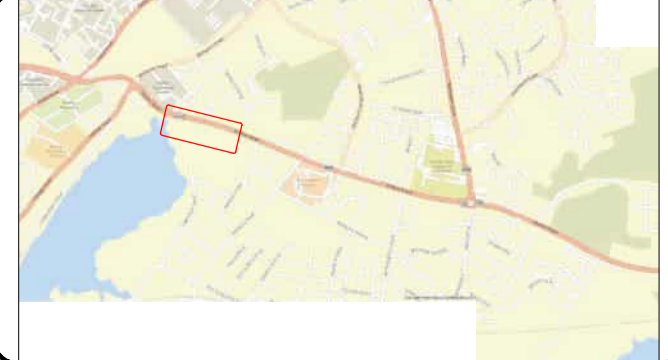
- BUS LANE
- CYCLE TRACK
- FOOTPATH
- CARRIAGEWAY
- GRASS AREA / VERGE
- PEDESTRIAN PRIORITY ZONE
- BUS STOP LOCATIONS
- EXISTING TREE TO BE RETAINED
- EXISTING TREE TO BE REMOVED
- PROPOSED TREES
- EXISTING BOUNDARY

GENERAL NOTES:

- DIMENSIONS NOT TO BE SCALED. WRITTEN DIMENSIONS ONLY TO BE USED.
- DRAWING SHOWS CONCEPTUAL ROAD LAYOUT BASED ON ORDNANCE SURVEY (OS) AND IS SUBJECT TO CHANGE.
- ROAD MARKINGS AS SHOWN ARE INDICATIVE ONLY.
- EXISTING AND PROPOSED STREET FURNITURE NOT SHOWN.
- PROPOSED BOUNDARIES/ FENCE TREATMENT WHERE EXISTING BOUNDARIES/ FENCES AFFECTED IS INDICATIVE ONLY.



© Copyright Ordnance Survey Ireland. All rights reserved. Licence No. 2022/21/CCMA/Galway City Council



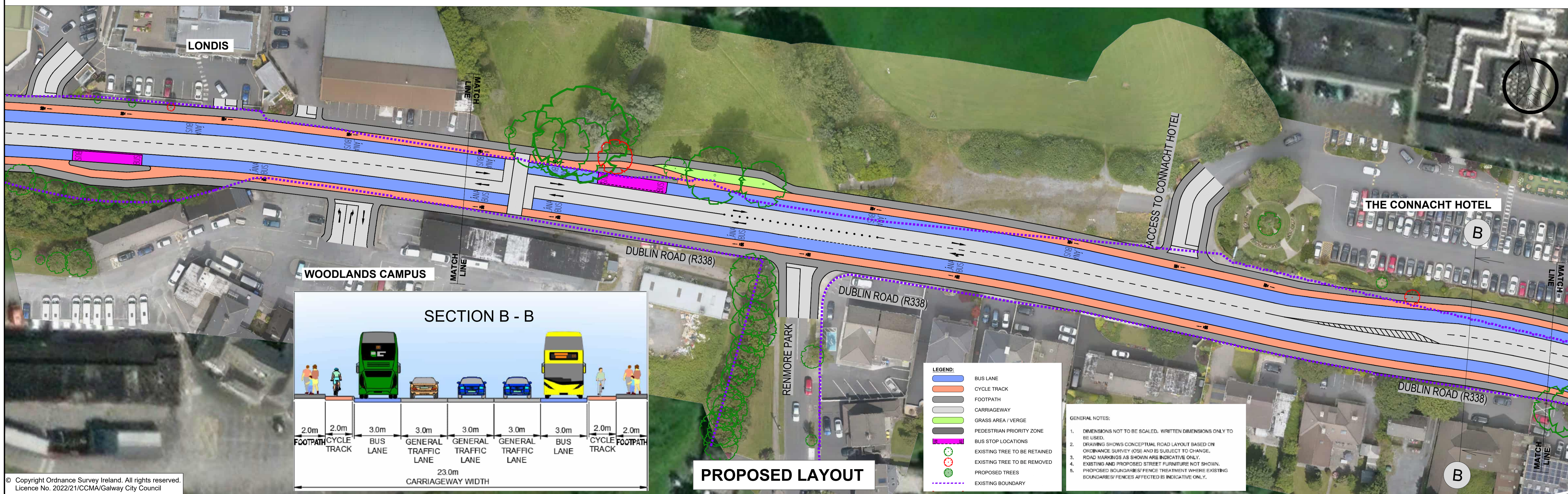
Notes:
DO NOT SCALE, use figured dimensions only. All levels are referred to Ordnance Survey Datum, Malin Head.
This drawing is the property of Barry Transportation. It is a confidential document and must not be copied, used, or its contents divulged without prior written consent. This document should not be relied on or used in circumstances other than those for which it was originally prepared. Barry Transportation accepts no responsibility for this document to any other party other than the party by whom it was commissioned.

Rev.	Date	Drawn	Description	Chk'd	Appr.
P02	16/11/22	WV	ISSUE FOR INFORMATION	RC	JN
P01	27/10/22	WV	ISSUE FOR INFORMATION	RC	JN

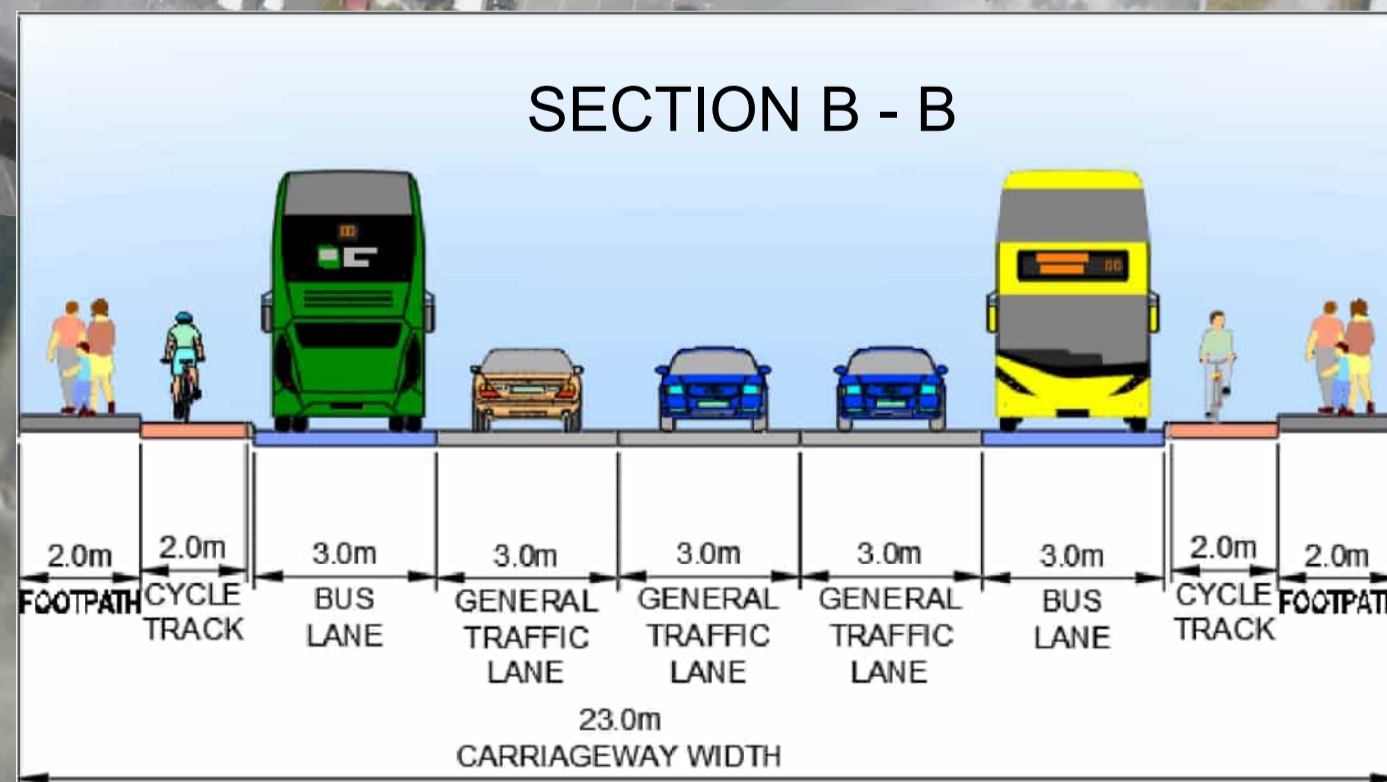
Project Title:	BUS CONNECTS GALWAY - DUBLIN ROAD	Status:	S3
Drawing Title:	MARTIN ROUNDABOUT R338 DUBLIN ROAD GENERAL ARRANGEMENT SHEET 01 OF 11	Rev:	P02
Designed: WV	Drawn No. 22047-BTL-ZZ-DR-CR-06010	Scale at A1: 1:500	Date: NOV 2022
Drawn: WV	Approved: JN	Reviewed: JN	



EXISTING LAYOUT



PROPOSED LAYOUT



LEGEND:

- BUS LANE
- CYCLE TRACK
- FOOTPATH
- CARRIAGEWAY
- GRASS AREA / VERGE
- PEDESTRIAN PRIORITY ZONE
- BUS STOP LOCATIONS
- EXISTING TREE TO BE RETAINED
- EXISTING TREE TO BE REMOVED
- PROPOSED TREES
- - - EXISTING BOUNDARY

- GENERAL NOTES:**
- DIMENSIONS NOT TO BE SCALED. WRITTEN DIMENSIONS ONLY TO BE USED.
 - DRAWING SHOWS CONCEPTUAL ROAD LAYOUT BASED ON ORDNANCE SURVEY (OS) AND IS SUBJECT TO CHANGE.
 - ROAD MARKINGS AS SHOWN ARE INDICATIVE ONLY.
 - EXISTING AND PROPOSED STREET FURNITURE NOT SHOWN.
 - PROPOSED BOUNDARIES/ FENCE TREATMENT WHERE EXISTING BOUNDARIES/ FENCES AFFECTED IS INDICATIVE ONLY.

© Copyright Ordnance Survey Ireland. All rights reserved.
Licence No. 2022/21/CCMA/Galway City Council

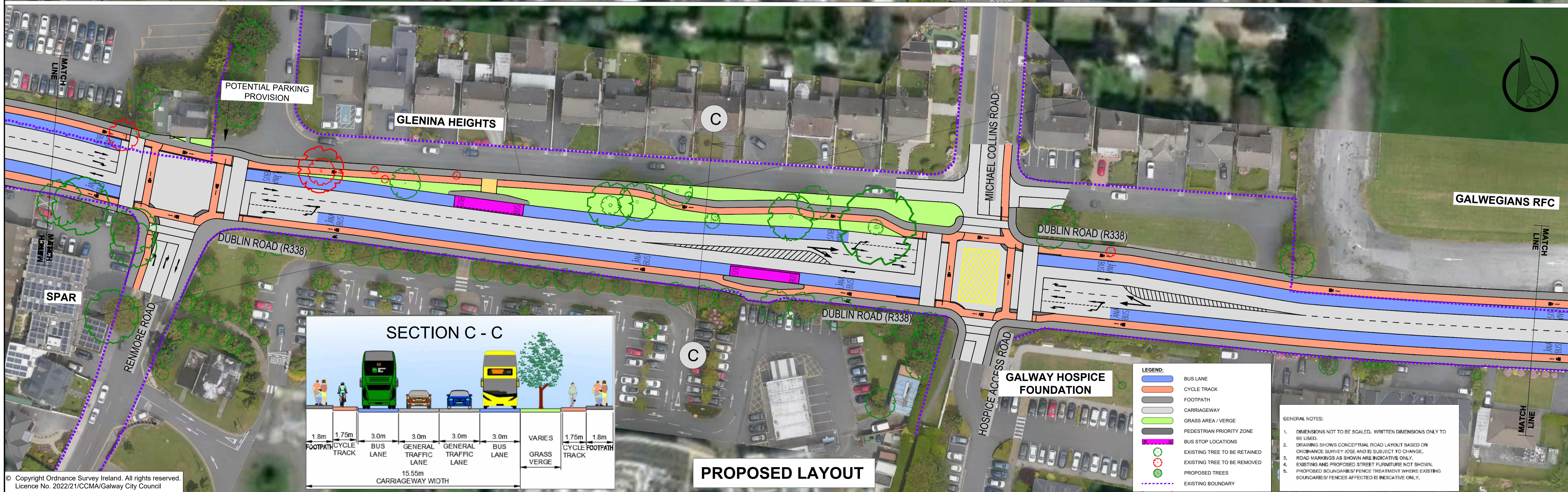


Notes:
DO NOT SCALE, use figured dimensions only.
All levels are referred to Ordnance Survey Datum, Malin Head.
This drawing is the property of Barry Transportation. It is a confidential document and must not be copied, used, or its contents divulged without prior written consent. This document should not be relied on or used in circumstances other than those for which it was originally prepared. Barry Transportation accepts no responsibility for this document to any other party other than the party by whom it was commissioned.

Rev.	Date	Drawn	Description	Chk'd	Appr.
P02	16/11/22	WV	ISSUE FOR INFORMATION	RC	JN
P01	27/10/22	WV	ISSUE FOR INFORMATION	RC	JN

Project Title:	BUS CONNECTS GALWAY - DUBLIN ROAD	Status:	S3
Drawing Title:	MARTIN ROUNDABOUT R338 DUBLIN ROAD GENERAL ARRANGEMENT SHEET 02 OF 11	Rev:	P02
Designed: WV	Drawing No.		
Drawn: WV	22047-BTL-ZZ-DR-CR-06009		
Approved: JN	Scale at A1: 1:500		
Reviewed: JN	Date: NOV 2022		

File Name: R332 Projects\22047 - BusConnects Galway - Dublin Road\00 GEN\020 Drawing\Separate.dwg\22047-BTL-ZZ-DR-CR-06009_Emerging_Prefined_Route 02.dwg



© Copyright Ordnance Survey Ireland. All rights reserved. Licence No. 2022/21/CCMA/Galway City Council

Tionscadal Éireann
Project Ireland
2040

BUS CONNECTS GALWAY
SUSTAINABLE TRANSPORT FOR A BETTER CITY.

NTA
Udarás Náisiúnta Iompair
National Transport Authority



Notes:
DO NOT SCALE, use figured dimensions only. All levels are referred to Ordnance Survey Datum, Mean Head.
This drawing is the property of Barry Transportation. It is a confidential document and must not be copied, used, or its contents divulged without prior written consent. This document should not be relied on or used in circumstances other than those for which it was originally prepared. Barry Transportation accepts no responsibility for this document to any other party other than the party by whom it was commissioned.

Rev.	Date	Drawn	Description	Chk'd	Appr.
P02	16/11/22	WV	ISSUE FOR INFORMATION	RC	JN
P01	27/10/22	WV	ISSUE FOR INFORMATION	RC	JN

Project Title:	BUS CONNECTS GALWAY - DUBLIN ROAD	Status:	S3
Drawing Title:	MARTIN ROUNDABOUT R338 DUBLIN ROAD GENERAL ARRANGEMENT SHEET 3 OF 11	Rev:	P02
Designed: WV	Drawn: WV	Approved: JN	Reviewed: JN
Drawing No.	22047-BTL-ZZ-DR-CR-06008	Scale at A1:	1:500
Date:	NOV 2022		

www.tobin.ie



TOBIN Consulting Engineers



@Tobinengineers

Sligo

Fairgreen House,
Fairgreen Road,
Sligo,
H91 AXK8,
Ireland.
Tel: +353 (0)91 565 211

Dublin

Block 10-4,
Blanchardstown Corporate Park,
Dublin 15,
D15 X98N,
Ireland.
Tel: +353 (0)1 803 0406

Castlebar

Market Square,
Castlebar,
Mayo,
F23 Y427,
Ireland.
Tel: +353 (0)94 902 1401